

St. Paul Union Depot Update



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**Capitol/Downtown Central
Corridor Task Force meeting
September 19, 2006**

What We Will Cover Today

- What the project is and what it isn't.
- What we hope to accomplish in St. Paul.



Central Corridor LRT simulation



Central Corridor LRT/Union Depot simulation

- Where we are in the process.
- The challenges we have faced and how we are overcoming them.

History of the Union Depot



Union Depot in the 1930s

- Built in 1923 to serve passenger and freight railroad for the upper Midwest. At its peak, the Depot served more than 200 passenger trains daily.
- Located in downtown St. Paul's historic Lowertown area, a vibrant neighborhood along the Mississippi River just 1 mile south of the State Capitol.

History of the Union Depot



Undated photo of Union Depot

- The once-bustling Depot closed its doors in 1971.
- The U.S. Postal Service, which owns the former concourse and adjacent track areas, expanded into the abandoned Depot.
- The Postal Service still uses the concourse for storage.

The Role of LOCATE

- LOCATE Task Force was formed in 2002 to identify a site for a multi-modal transit center in downtown St. Paul.
- Its members include elected officials, businesses and transit providers, including Amtrak and Greyhound.



Rendering of renovated concourse

The Role of LOCATE

- 2002 — the first of two formal studies looked at eight sites.
- The former Depot site in St. Paul's Lowertown was chosen.
- Well over 100 years ago, the railroads chose the same riverfront site for their Depot — and it is where the transit infrastructure was built.



The role of LOCATE



Two views of renovated Depot

- 2003 — A second study tackled logistics of the redevelopment.
- The conclusion: The current Depot site could form the heart of a multi-use transportation center featuring Amtrak, commuter rail, light rail, high-speed rail, inter-city buses and Metro Transit services.

The Union Depot Today



Current view of concourse interior



Current view of concourse exterior

- No passenger or freight trains serve Union Depot.
- The Postal Service has agreed to move operations out of the Depot and consolidate into a suburban facility, but many challenges remain.

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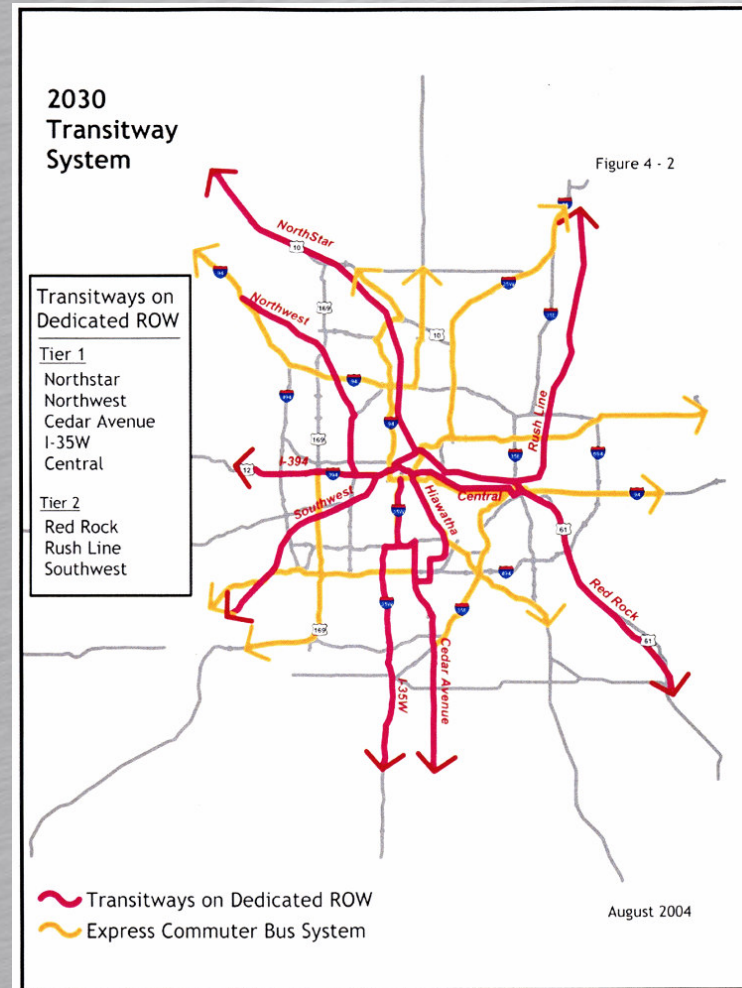
The Union Depot Today



- The Rail Authority is developing the concourse and track areas and is in the process of acquiring the necessary land.
- The Rail Authority is seeking an easement through the privately owned Headhouse (above).
- The Headhouse is currently used for offices and restaurants. Construction of condominiums is under way.

The Vision for the Union Depot

- The Union Depot will once again become a destination center of the Twin Cities.
- The Twin Cities regional transportation plan includes several transit corridors converging at the Depot.



The Vision for the Union Depot

Amtrak

- Moving operations from St. Paul's Midway Station to the Depot.



The Vision for the Union Depot

LRT

- Central Corridor
- Robert Street Transitway



The Vision for the Union Depot

Commuter Rail

- Red Rock Corridor
- Rush Line



The Vision for the Union Depot

Buses

- Greyhound
- Jefferson Lines
- Metro Transit



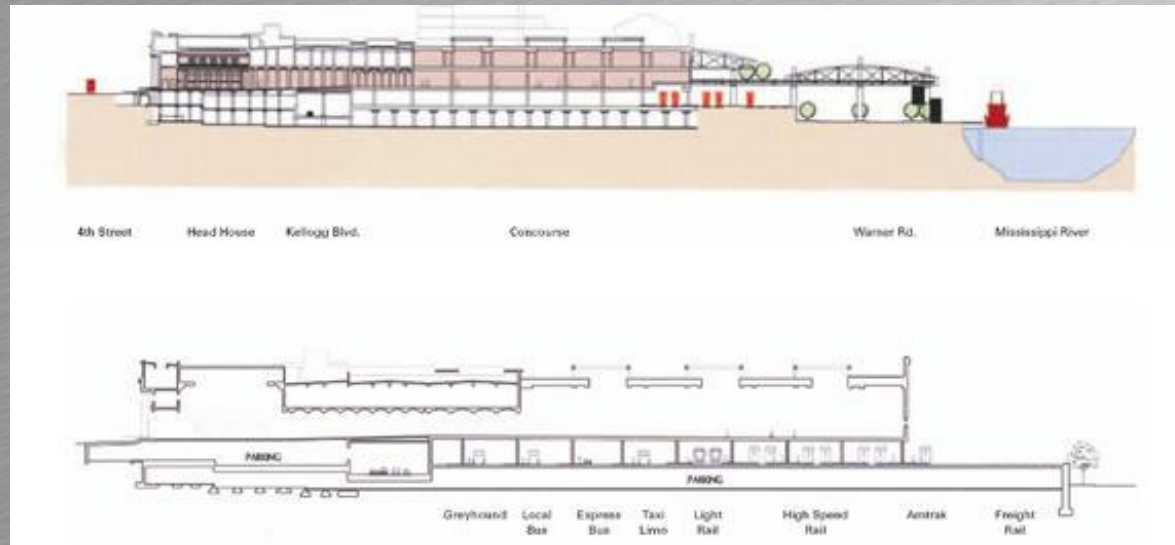
The Vision for the Union Depot

High Speed Rail

- Midwest high-speed rail from Chicago to the Twin Cities



The Next Steps



Cut-away of Depot renovation plans

- Funding for infrastructure near new suburban postal facility, clearing the way for USPS to move from Union Depot.
- Purchase concourse from Postal Service.
- Environmental assessment of Depot and adjacent property.

The Next Steps

- A request for proposal was issued in August for initial Phase 1 work:
 - Conceptual Engineering
 - Financial plan
 - Environmental assessment
- A contractor will be selected this fall
- Phase 1 includes:
 - Land acquisition and infrastructure improvements to concourse
 - Infrastructure for running railway lines through the Depot
 - Move of Amtrak, Greyhound and Jefferson Lines stations to Union Depot and necessary infrastructure work
 - Construction of Central Corridor LRT station
 - Metro Transit bus infrastructure

The Costs

Multi-modal Investment

- Phase 1: Amtrak, Intercity buses, LRT, Metro Transit — \$100 million, plus land-acquisition costs still being negotiated
- Future Phases: Commuter and High-Speed Rail

Total: \$361 million

(cost estimates in year of expenditure dollars)

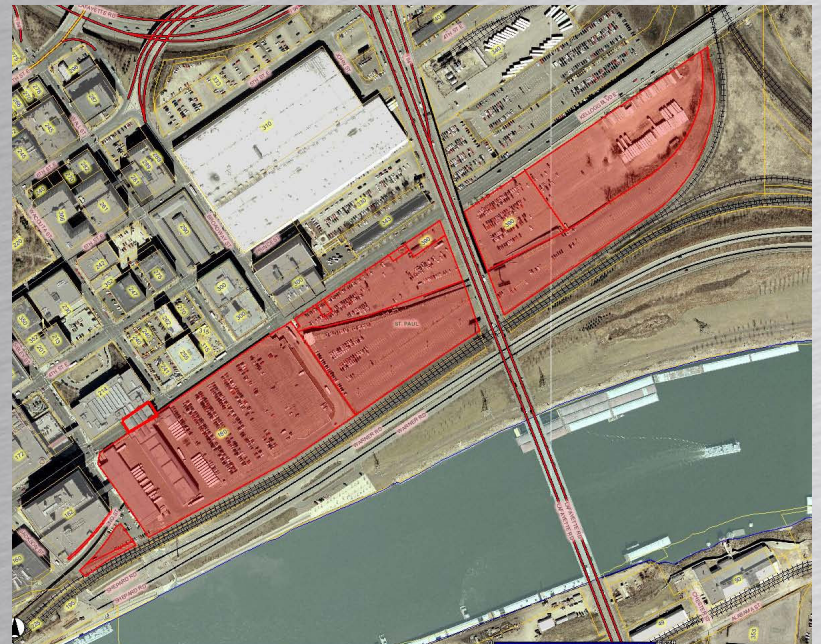
How We Will Pay For It

Funding Commitments

- \$53.4 million in federal funding
- \$5 million in Rail Authority land purchases to date
- \$3.5 million in state bonding

Still Needed

- Additional funds from state of Minnesota; Rail Authority has requested \$9 million in current bonding bill
- Funding from rail corridor partners
- Additional bonds to be floated by Rail Authority



Aerial view of project area

For More Information



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